

WARD: Lockleaze CONTACT OFFICER: Andrew Cross

SITE ADDRESS: 17 Bridge Walk Bristol BS7 0LE

APPLICATION NO: 17/06631/F Full Planning

DETERMINATION DEADLINE: 10 August 2018

Demolition of existing dwelling and construction of a three storey block of 7 apartments with associated car parking, cycle and refuse storage.

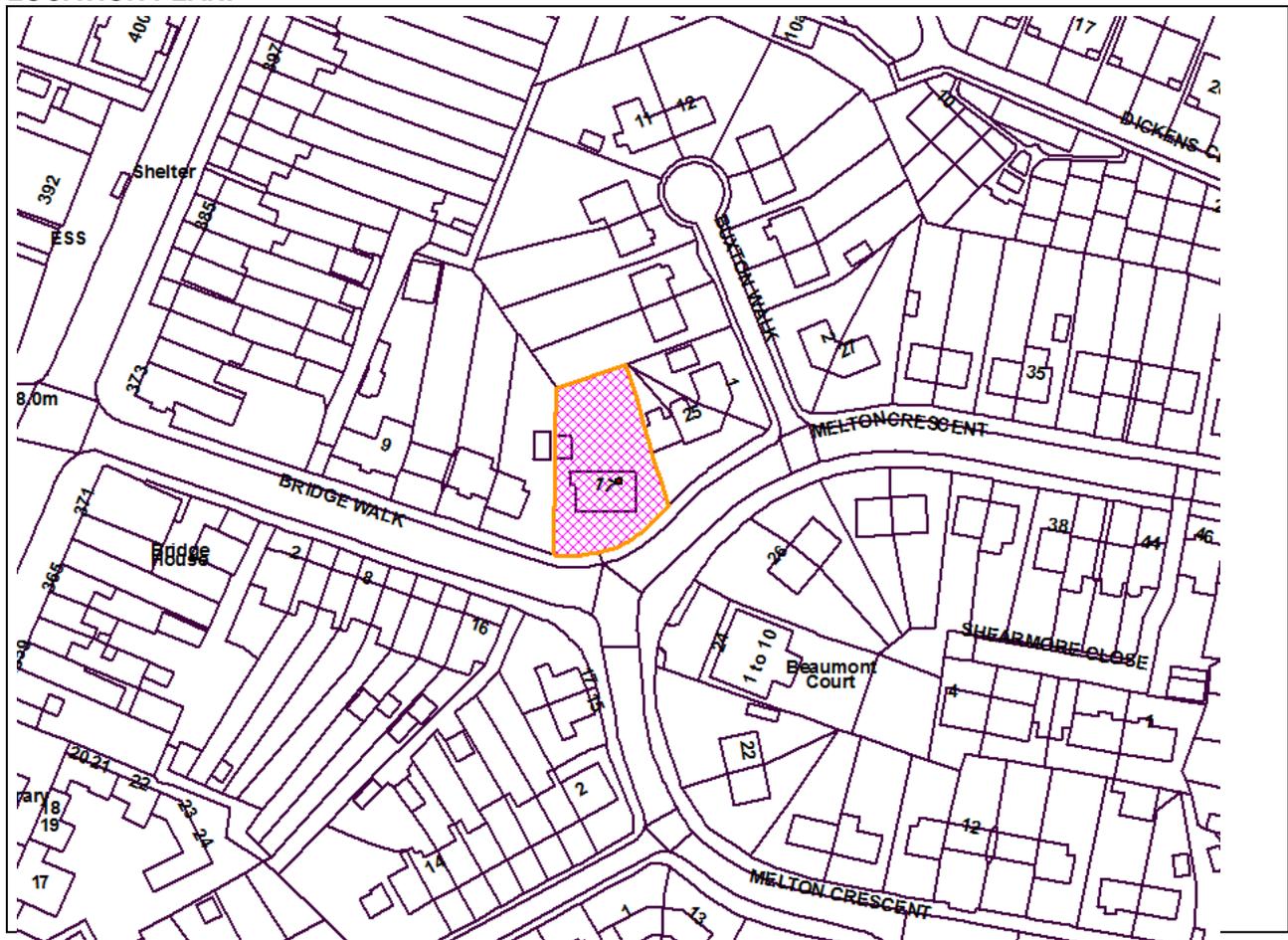
RECOMMENDATION: Grant subject to Condition(s)

AGENT: Aspect360 Ltd
45 Oakfield Road
Clifton
Bristol
BS8 2AX

APPLICANT: Mr J. Fedrick
17 Bridge Walk
Bristol
BS7 0LE

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



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SITE DESCRIPTION AND APPLICATION

The application relates to a plot of land, currently occupied by a detached bungalow, on the northern side of Bridge Walk, Lockleaze. The plot fronts Bridge Walk opposite the junction with Melton Crescent.

The application proposes the demolition of the existing bungalow on the site and construction of a three-storey building containing seven flats. The existing vehicular access point would be retained, serving one off-street parking space; and providing access to communal cycle storage and refuse/recycling storage facilities. Landscaped areas around the building would provide private and communal amenity areas.

See plans and supporting documents for full details.

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RELEVANT HISTORY

There is no relevant planning history at the application site.

EQUALITIES ASSESSMENT

During the determination of this application due regard has been given to the impact of this scheme in relation to the Equalities Act 2010 in terms of its impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. There is no indication or evidence (including from consultation with relevant groups) that different groups have or would have different needs, experiences, issues and priorities in relation this particular proposed development. Overall, it is considered that the approval / refusal of this application would not have any significant adverse impact upon different groups or implications for the Equalities Act 2010.

RESPONSE TO PUBLICITY AND CONSULTATION

Consultation was undertaken via site notice and letters sent to surrounding properties. 12 written responses were received from surrounding residents, along with representation from local councillors.

Councillors Tincknell and Kirk have commented as follows:

We would like to register our concerns about the above application and to reflect those of ward residents in the area. The proposed building is in design out of keeping with the style and size of adjacent buildings, which are mainly residential and 'traditional' in design. It is much larger than neighbouring properties and is likely to encroach on the privacy and access to sunlight of houses next to it. There are also concerns about restricted parking spaces in the area and the impact this block would have, especially at busy times. We consider this application would lead to overdevelopment of this residential street.

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Concerns raised by surrounding residents can be summarised as follows:

Residential Amenity

- o Overbearing on neighbouring buildings and residents
- o Overlooking
- o Overshadowing
- o Noise associated with increased occupancy
- o Noise from vehicle accessing the rear of the site
- o Noise and disturbance during construction
- o Potential for refuse/recycling issues

Design

- o Development is over-scaled for the context
- o The building design is out of character for the area
- o Inappropriate materials
- o Building is set too far forward, out of context of existing building lines
- o Overdevelopment of the site
- o Lack of spacing between properties
- o Loss of green (garden) space

Highways

- o Highway Safety, including pedestrian safety
- o History of accidents due to poor visibility in the vicinity of the site
- o Increased pressure for on street parking
- o Inadequate parking provision on site

Flood Risk

- o Flooding concerns as rainwater would not connect to main sewers [the system proposed would connect to the main sewer]

Other

- o Loss of views

A second round of consultation was undertaken following minor revisions to the scheme. 12 written responses were received from neighbouring residents, reiterating comments previously made as set out above.

Councillor Kirk commented as follows:

Despite some revision of the original plans, I still believe this proposed development to be inappropriate and disproportionately large for this residential street. Close neighbours have expressed their opposition on grounds of the new development overlooking their gardens, infringing on their privacy, blocking light and compromising road safety with a lack of off road parking that is likely to add to pre-existing parking pressures in this road. I share these concerns and do not feel the revised plans address the problems raised by residents.

Following more significant revisions to the scheme, which amended the layout, siting, design and form of the proposed development, a third round of consultation was undertaken, via site notice and neighbour letters. Nine written responses were received, the comments received again reiterate the concerns previously raised, as set out above.

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The various points raised will be discussed within the key issues section of this report.

The City Council Urban Design Officer has commented in relation to the original submission as follows (25.01.2018):

- o There is too much bulk at high level - this should be reduced.
- o A family unit with a garden is required.
- o Move bin store off frontage.
- o Direct access to/from the street should be provided, rather than side/rear access.
- o Red brick (not buff) should be used to accord with local character.
- o A good quality landscaping scheme is required.

Some amendments were made to the scheme, with further comments as follows (24/04/18 and 01/05/2018):

A combination of the spatial qualities of site; the proposed brutal architecture; an over-complicated design; and lack of soft landscaping, results in a scheme that does not sit comfortably within context appearing harsh and contrary to design policies.

The building form should be simplified and the front building line pushed back from the street, with vegetation to visually soften. Ground floor units should have direct (principle) access from street. Increase glazing to south elevation - relocate living spaces and bedrooms to aid privacy as part of this. Could have front porch(es) to frame entrance(s) off street and break up the south elevation. Points previously raised prior to amendments are not fully resolved - rear parking, front entrances.

A revised scheme was received 11/07/18, with some further minor amendments requested regarding layout, prior to the submission of the final scheme for consideration.

The City Council Urban Design Officer has commented as follows (23.10.2018):

It is considered that the revised scheme positively addresses the majority of issues raised, as such City Design Group do not object on design grounds.

The City Council Highways Officer has commented as follows (09.11.18):

Recommendations

Transport Development Management considers the revised proposal acceptable on highway safety grounds providing:

- o A site plan with vehicular visibility splays of 2.4m x 25m (20mph) is submitted to demonstrate that the proposed location of the building will not obstruct motorist's views of any oncoming traffic.
- o A Parking Survey is undertaken to determine if there would be sufficient room on-street to support any additional vehicles generated by residents of the development.
- o Appropriate drainage is provided at the point of access.
- o Double yellow lines are provided in front of the site to prevent vehicles from parking on the footways, thereby obstructing vehicular visibility splays.
- o The size of the cycle store is increased to enable 14 cycles to be accommodated.
- o The waste and cycle store area and the driveway are appropriately illuminated.

Conditions recommended regarding parking provision, servicing facilities, external lighting, and retention of a clear visibility splay to a height of 0.6 metres.

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The City Council Nature Conservation Officer has commented as follows (06.02.18):

Conditions are recommended with regard to a 'soft strip' method being employed for roof removal of the existing property to be demolished, as a precautionary measure in accordance with recommendations in the submitted bat scoping survey.

The proposed site plan appears to show the retention of the existing hedgerows on the western and northern boundaries and this is recommended from an ecological point of view. A condition is recommended in relation to clearance of any vegetation to avoid the bird nesting season.

In accordance with Policy DM29 in the Local Plan, the provision of a living (green/brown) roof is recommended to provide habitat for wildlife. Policy DM29 states that 'proposals for new buildings will be expected to incorporate opportunities for green infrastructure such as green roofs, green walls and green decks.' Living roofs can be integrated with photovoltaic panels and also contribute towards Sustainable Urban Drainage Systems (SuDS), air pollution mitigation and reducing the urban heat island effect. Living roofs can be provided on buildings, as well as on bin stores and cycle shelters. [Guidance for living roofs is provided within the Advices section of this report].

RELEVANT POLICIES

National Planning Policy Framework – July 2018

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017.

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES

(A) PRINCIPLE AND HOUSING MIX

Core Strategy policy BCS5 aims to deliver new homes within the built up area to contribute towards accommodating a growing number of people and households in the city. The policy states that the development of new homes will primarily be on previously developed sites across the city, but some new homes will be developed on open space which does not need to be retained as part of the city's green infrastructure provision. Policy BCS20 seeks the efficient use of land, with higher densities supported in various locations including in and around designated centres, and along or close to main public transport routes.

The application site is located approximately 50 metres from the designated local centre at Filton Avenue, along with regular bus services to/from Filton Avenue. A larger designated centre is also located approximately 550 metres to the west on Filton Road. On this basis the provision of higher density development on the site is supported in principle terms by current policy.

The scheme would provide 1 one-bedroom unit, 5 two-bedroom units, and 1 three-bedroom family unit. Each flat would have private outdoor amenity space, which in the case of the family unit would comprise a private garden to the rear of the site, whilst the other flats would benefit from balconies/terraces. A communal garden is also proposed to the rear of the site.

Housing stock within the Lockleaze ward (2011 census) comprises 17.5% flats and 82.5% houses. 70.6% of the dwellings contain 3 or more bedrooms. The application site is located within the Filton Avenue North Lower Super Output Area (LSOA) which comprises 28.4% flats and 71.6% houses. 53% of the dwellings contain 3 or more bedrooms. The adjoining LSOA at Filton Avenue South has a

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higher proportion of houses at 93%, and 86.8% of the accommodation has 3 or more bedrooms. On the basis of these figures the principle of flats is acceptable, and overall it is considered that a reasonable accommodation mix would be provided.

(B) RESIDENTIAL AMENITY

Policy BCS18 requires residential development to provide sufficient space for everyday activities and enable flexibility and adaptability by meeting appropriate space standards. Policy BCS21 expects development to create a high quality environment for future occupiers while safeguarding existing surrounding development. Policy DM2 requires development to provide a good standard of accommodation by meeting relevant requirements and standards. Policy DM29 expects new buildings to safeguard the amenity of the host premises and neighbouring occupiers.

During the course of the application a number of design revisions were undertaken, which included consideration of the relationship with neighbouring properties and residents. Whilst new windows and terraces/balconies to the front and rear of the building would enable additional overlooking of surrounding properties when compared with the existing bungalow, this would be to a degree that conforms with the established layout characteristics of the area and is reasonable within the context. Side-facing windows to habitable rooms at first and second floor levels would all be secondary windows to rooms and are proposed to be obscure glazed to prevent overlooking, which is acceptable and can be secured by condition.

Balconies/terraces are proposed to the front of the building and are generally enclosed, with the exception of the top floor unit which would have a terrace at roof level. A condition would be added requiring the provision of privacy screens either end of the top floor terrace to prevent overlooking of private garden areas. A condition would also be imposed, preventing the use of the rear portion of the building's roof being used as a terrace/balcony in order to safeguard neighbouring amenity.

The proposed building would have a larger footprint and would be of a greater scale than the existing bungalow on site, and as such the proposed building would have a greater impact than existing when considering overshadowing. The proposal is supported by the provision of a shadow study, which details that the level of shading of neighbouring properties would be greater than existing, as expected. The position of the building in relation to neighbouring properties has however been considered in relation to shading as part of the design process and the extent of shading is limited to partial shading of neighbouring gardens at certain times at certain times of the year. Overall the overshadowing impacts are considered limited and acceptable in residential amenity terms.

The position of the building within the site has taken account of building lines to the front, as well as relationship with the rear of neighbouring properties. The ground floor would be set into the ground slightly, and the top storey would not cover the full extent of the building. The result of these measures are in part that the scale and bulk of the building are minimised, particularly where development is adjacent to neighbouring properties, and as such the development would not be unacceptably overbearing upon neighbouring residents.

Noise associated with increased occupancy was raised as a concern within objection comments. Amendments to the scheme during the course of the application have removed the originally proposed parking area from the rear of the site, and replacing this with private and communal soft landscaped garden space. This arrangement accords with the existing character and layout of land use/function and prevents intrusion upon surrounding private garden areas that would result from noise associated with vehicle movements. Whilst the number of residents occupying the site would increase, the residential occupation of the building proposed does not give rise to noise concerns within the surrounding residential context.

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The flats are to be arranged with living rooms to the front and bedrooms to the rear, with associated benefits of natural surveillance of the public realm from living rooms, and privacy to bedroom areas which would be located away from the public realm.

When considering future residents, the proposed dwellings accord with current space standards requirements, and with all flats benefitting from private outdoor amenity space (a garden for the family unit and a balcony/terrace for each other unit) as well as the communal garden space available for use by all residents. Servicing facilities are conveniently located. All but two units are dual aspect, and those which are single-aspect face south-east such that reasonable levels of daylight would be achieved.

A Construction Environmental Management Plan could be conditioned as part of any approval to ensure that disruption is minimised during construction.

On the basis of the above, and given the imposition of relevant planning conditions, the proposal is found to be acceptable in residential amenity terms.

(C) DESIGN

Core Strategy policy BCS21 relates to overarching urban design principles to ensure high quality development acceptable within its context, with Development Management policies DM21 and DM26 relating specifically to the development of private gardens and local character and distinctiveness respectively. Policy DM26 requires development to contribute towards local character and distinctiveness, in relation to various factors including pattern and grain of development, scale, character, function and architectural styles. Policy DM27 is concerned with layout and form, including principles in relation to blocks and plots. DM29 relates to new buildings and requires a high standard of quality and visual interest which contributes positively to the character of an area.

The application site is set within an existing residential area, amongst a post-war housing development. Further to the west is more historic housing stock, while to the north is a modern housing estate. The immediate context is defined by frontage development set within irregular-shaped blocks with occasional intervening cul-de-sacs. The locally characteristic building stock is of predominantly two-storey terraced and semi-detached houses, although there are exceptions including the existing detached bungalow which occupies the application site, and a larger flatted block opposite. Hipped and gable-ended roofs are commonplace locally, with prevailing finishing materials being a combination of red facing brickwork and render and red/brown roofing tiles.

The building proposed is of three stories, with the ground floor set into the ground slightly so that the overall building height accords with that established locally. The proposed development is of a greater scale and bulk than surrounding development, however significant improvements in this regard have been made during the course of the application, through a reduction in high level bulk. The second floor is set within a roof structure set back from the building edge, to further reduce height level bulk.

The building is of a contemporary form and of a more brutal architectural appearance than surrounding development, however elevational relief and vertical features, along with improved fenestration layout, and the provision of soft landscaping, have softened the building when compared with the original submission. The building has also been pushed back into the site when compared with the original submission, with the effect of reducing visual dominance within the street scene and better according with established building line and layout characteristics.

It is acknowledged that the building is of a greater scale than surrounding development, however the application plot is also of more generous dimensions, and the design approach has sought to provide a unique building that also draws on established local characteristics in relation to layout and

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materiality, with the building to be predominantly finished in red facing brickwork.

The proposed design is considered to incorporate adequate breathing space between existing and proposed buildings and the development includes private and communal garden space as well as private terrace areas serving each flat.

Overall the development is considered to represent an efficient use of the site that responds to local design characteristics whilst providing a unique contemporary building. Subject to the imposition of relevant planning conditions, the proposed design is considered acceptable.

(D) HIGHWAYS AND SERVICING

Core Strategy policy BCS10 sets out a transport hierarchy for the design of developments, and expresses that development should be located where sustainable travel patterns can be achieved; should minimise the need to travel; and maximise opportunities for the use of walking, cycling and public transport. It is also expressed that developments should be designed and located to ensure the provision of safe streets.

Policy DM23 expresses that development should not give rise to unacceptable traffic conditions and will be expected to provide: safe and adequate access onto the highway network; adequate access to public transport; transport improvements where necessary; adequate provision for pedestrians and cyclists. The policy also requires the provision of adequate servicing facilities, and safe accessible and usable parking in accordance with the parking standards schedule. Policy DM32 requires adequate refuse and recycling provision in new development.

Throughout the planning application process a number of alterations to the scheme design have occurred from a highways perspective, including alterations to the parking and servicing arrangements and layout, which has included the reduction in off-street parking to one space, with associated turning head to enable a vehicle to enter and exit the site in a forward gear. Neighbour comments have raised concern of inadequate off-street parking provision, however the level of off-street parking proposed accords with current planning policy requirements, which set a maximum acceptable level rather than stipulating a minimum necessary provision. It should also be noted that the site is considered to be sustainably located by virtue of its position approximately 60 metres from local shopping provision within a designated local centre, and being well served by frequent bus services.

The single parking space would be served by a turning head so that inward and outward vehicle manoeuvres would be in a forward gear, with the existing vehicular access point being used. The City Council Highways Officer has expressed that a 2.4 by 25 metre visibility splay should be provided from the site exit, which is possible to the east, and any structures/vegetation within that splay be no more than 0.6 metres in height, which would affect the landscaping scheme and can be conditioned. To the west the existing neighbouring hedge would be within the aforementioned visibility splay, however this is as existing and outside the application site such that it cannot be controlled under the scope of this application.

The City Council Highways Officer has requested a parking survey be undertaken for the area to aid assessment as to whether the proposed parking levels are acceptable. Given that the proposal is sustainably located and that the level of car parking proposed is policy compliant, this is not considered reasonable or necessary. The suggested provision of double yellow lines to the front of the property is also considered unreasonable under the scope of the application given that the existing established vehicular access/exit point would be retained; with fewer vehicle movements into/out of the site likely due to a reduction in parking provision; and given that parking can currently take place on-street in this location, with the same relationship with the existing vehicular access.

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Cycle storage would be provided within a purpose store to the east of the building within a communal area. The location and layout of the cycle store are acceptable. 12 cycle parking spaces are proposed, however a policy compliant provision would be 13 spaces, and as such an amended cycle store could be secured by condition to ensure policy compliant cycle parking provision.

Refuse and recycling would be stored within a purpose built store, which is conveniently located and adequately sized. External lighting can be secured by condition to ensure a safe environment around the servicing facilities.

In the interests of Highway Safety and general amenity, it is recommended that a Construction Management Plan be secured by condition.

On the basis of the above, subject to the provision of relevant planning conditions the proposal is found acceptable in relation to highways and servicing matters.

(E) SUSTAINABILITY AND FLOOD RISK

Current planning policy (BCS13-16) within the adopted Bristol Development Framework, Core Strategy (2011) requires new development to be designed to mitigate and adapt to climate change and meet targets to reduce carbon dioxide emissions. This should be achieved, amongst other measures, through efficient building design, the provision of on-site renewable energy generation to reduce carbon dioxide emissions by at least 20% based on the projected residual energy demand of new buildings and extensions to existing buildings, and for new development to mitigate against the risk of flooding, including rainwater soak-away drainage. The approach proposed should also be supported by the provision of a sustainability statement and an energy strategy.

On-site renewable energy generation is proposed through the use of solar PV panels, adequate to reduce CO2 emissions from residual energy demand by 22%, which is acceptable and policy compliant.

Rainwater soak-away drainage is not feasible on site, and following a hierarchical approach, underground storage tanks are proposed, with attenuated flow feeding into the main sewer in order to limit discharge rates to 5 litres per second. Filtration to achieve water quality improvements is also to be incorporated. A green roof is also proposed to the rear portion of the roof above first floor level, and to the second floor roof, which would aid a reduction in run-off rates. These measures are acceptable. A detailed design for a sustainable drainage system to serve the development would be required if permission is granted, which could be secured via condition.

Given the imposition of appropriate planning conditions, the proposal is found to be acceptable when considering issues relating to sustainability and flood risk.

(F) TREES AND LANDSCAPING

Policies BCS9 and DM17 seek to retain existing trees wherever possible as part of new development. Policy BCS9 expresses that development should incorporate new and/or enhanced green infrastructure appropriate for the site. Policy BCS21 requires development to provide a high quality environment for future occupiers and DM27 expects a high quality landscape design including consideration of functionality of external spaces as well as the use of trees and other plants appropriate to the character of the site and its context.

The proposal would not result in the loss of any trees and existing hedges to the northern and western boundaries would be retained. Whilst the extent of garden space would be reduced when compared with existing, private and communal soft landscaped gardens would be provided, as well as useable

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hard surfaced servicing areas. A green roof would also provide biodiversity benefits. The scheme includes the provision of soft landscaping to the front of the building to soften the form of the building and in recognition of the character of the area, which includes soft landscaped frontage areas. A scheme of soft landscaping for the site will need to be conditioned as part of any approval for the site, and in relation to the frontage area will need to take account of the visibility splay set out within key issue D.

On the basis of the above and given the imposition of relevant planning conditions, the proposal is considered acceptable in relation to trees and landscaping.

(G) NATURE CONSERVATION

Core Strategy policy BCS9 states that national and local sites of biological and geological conservation importance will be protected having regard to the hierarchy of designations and the potential for appropriate mitigation. The extent to which a development would contribute to the achievement of wider objectives of the Core Strategy will be carefully considered when assessing their impact on biological and geological conservation. Where development would have an impact on the Bristol Wildlife Network it should ensure that the integrity of the network is maintained or strengthened.

The proposal incorporates a significant area of green/living roof, which would provide wildlife benefits and help mitigate the loss of existing soft landscaped garden area. A condition would be added for the provision of the proposed green/living roof prior to occupation and advice given in relation to an appropriate specification.

The application is supported by a bat scoping survey, and whilst no evidence of bats has been found on site, the City Council Nature Conservation Officer has recommended that the roof of the existing building is removed using a 'soft strip' method as a precautionary measure, as set out within the recommendations of the submitted bat survey. This could be secured by condition.

Overall, the proposal is considered acceptable in nature conservation terms.

CONCLUSION

The proposal would provide appropriately positioned and required residential accommodation which meets city wide and localised policy aspirations. The scheme adequately safeguards neighbouring amenity as well as that of future occupiers and is of a design which, whilst has given rise to some concern locally, has been amended to successfully resolve initial concerns raised from Urban Design Officers, and would provide a striking modern addition to the area. Adequate parking and servicing arrangements would be provided, in line with current planning policy requirements, with the existing established vehicular access utilised. The scheme adopts a sustainable design approach and incorporates renewable energy technologies, as well as a sustainable drainage system. As such the application is recommended for approval, subject to relevant planning conditions, as set out below.

COMMUNITY INFRASTRUCTURE LEVY (CIL)

CIL LIABILITY

The CIL liability for this development is £29,660.71

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RECOMMENDED GRANTED subject to condition(s)

Time limit for commencement of development

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. Site Specific Construction Environmental Management Plan

No development shall take place until a site specific Construction Environmental Management Plan has been submitted to and been approved in writing by the Council, and be adhered to throughout the demolition and construction periods. The plan should include, but not necessarily be limited to:

Parking of vehicle of site operatives and visitors

Routes for construction traffic

Hours of operation

Method of prevention of mud being carried onto highway

Pedestrian and cyclist protection

Proposed temporary traffic restrictions

Arrangements for turning vehicles

Arrangements to receive abnormal loads or unusually large vehicles

Procedures for maintaining good public relations including complaint management, public consultation and liaison

Arrangements for liaison with the Council's Pollution Control Team

All works and ancillary operations which are audible at the site boundary, or at such other place as may be agreed with the Local Planning Authority, shall be carried out only between the following hours:

08 00 Hours and 18 00 Hours on Mondays to Fridays and 08 00 and 13 00 Hours on Saturdays and; at no time on Sundays and Bank Holidays.

Deliveries to and removal of plant, equipment, machinery and waste from the site must only take place within the permitted hours detailed above.

Mitigation measures as defined in BS 5528: Parts 1 and 2 : 2009 Noise and Vibration Control on Construction and Open Sites shall be used to minimise noise disturbance from construction works.

Procedures for emergency deviation of the agreed working hours.

Bristol City Council encourages all contractors to be 'Considerate Constructors' when working in the city by being aware of the needs of neighbours and the environment.

Control measures for dust and other air-borne pollutants. This must also take into account the need to protect any local resident who may have a particular susceptibility to air-borne pollutants.

Measures for controlling the use of site lighting whether required for safe working or for security purposes.

Reason: In the interests of the amenities of surrounding occupiers

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3. Sustainable Urban Drainage System (SUDS)

No development shall take place until a detailed design of surface water drainage for the site using sustainable drainage methods has been submitted to and approved in writing by the Local Planning Authority. The scheme of surface water drainage shall be completed in accordance with the approved detailed design prior to the occupation of the building and shall be retained and maintained as such thereafter.

Reason: To ensure that the principles of sustainable drainage are incorporated into this proposal.

4. Submission and Approval of Landscaping Scheme

No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a scheme of hard and soft landscaping for the whole site, which shall include indications of all existing trees and hedgerows on the land, and details of any to be retained, together with measures for their protection, in the course of development. The landscaping scheme shall take account of the need for a 2.4 metre by 25 metre visibility splay from the vehicular egress point, and within that splay there shall be no plants or structures in excess of 0.6 metres in height. Elsewhere on the frontage area more substantial planting is encouraged where appropriate. The approved scheme shall be implemented so that planting is carried out no later than the first planting season following the occupation of the building(s) or the completion of the development whichever is the sooner. All planted materials shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To protect and enhance the character of the site and the area, and to ensure its appearance is satisfactory.

5. Green roof specification and provision

Prior to the commencement of the green roof hereby approved, detailed specifications of the green roof construction and planting proposed shall be submitted to and approved in writing by the local planning authority. The green roof shall then be completed in accordance with the approved details prior to the first occupation of the development hereby approved, and retained and maintained as such thereafter.

Reason: In the interests of sustainability, flood risk and biodiversity.

6. Sample Panels before specified elements started

Sample panels of all external finishing materials are to be erected on site and approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved details before the building is occupied.

Reason: To ensure an appropriate finished appearance within the context.

7. Bats

As a precautionary measure, the demolition of the bungalow shall be carried out using a 'soft strip' method. The roof shall be carefully stripped by hand and roof tiles and panels should be

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lifted not slid, in order to prevent any potential injury to bats. If bats are encountered all demolition or construction work should cease and an ecological consultant or the Bat Conservation Trust (Tel 0845 1300 228) should be consulted for advice.

Reason: To conserve legally protected bats in the event that they are found to be roosting.

8. Vegetation Clearance

No clearance of vegetation or structures suitable for nesting birds, shall take place between 1st March and 30th September inclusive in any year without the prior written approval of the local planning authority. The authority will require evidence provided by a suitably qualified ecological consultant that no breeding birds would be adversely affected before giving any approval under this condition. Where checks for nesting birds by a qualified ecological consultant are required they shall be undertaken no more than 48 hours prior to the removal of vegetation or the demolition of, or works to buildings.

Reason: To ensure that wild birds, building or using their nests are protected.

9. Artificial Lighting (external)

Prior to the commencement of the relevant element of the scheme, a report detailing an external lighting scheme for the development, to include dusk to dawn lighting, and including predicted light levels at neighbouring residential properties, shall be submitted to and approved in writing by the Local Planning Authority. The scheme of external lighting shall then be completed in accordance with the approved details prior to the first occupation of the development hereby approved.

Artificial lighting to the development must conform to requirements to meet the Obtrusive Light Limitations for Exterior Lighting Installations for Environmental Zone - E2 contained within Table 1 of the Institute of Light Engineers Guidance Notes for the Reduction of Obtrusive Lighting, GN01, dated 2005.

Reason: In the interests of safety and security and in order to safeguard the amenities of surrounding residential occupiers

10. Privacy screens

Full details of privacy screening/guarding to each end of the roof terrace serving the second floor flat shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of that element of the scheme. The details approved shall be completed prior to the occupation of the development and retained and maintained as such thereafter.

Reason: In the interests of residential amenity.

11. Cycle Storage

Prior to the commencement of the cycle storage facilities, full details of the proposed cycle store shall be submitted to and approved in writing by the Local Planning Authority, and shall be adequate to accommodate at least 13 bicycles. The development shall not be occupied until the cycle parking facilities have been provided in accordance with the approved details and thereafter shall be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

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12. Refuse and Recycling Storage

Prior to the construction of the refuse and recycling storage facilities, full details of the proposed refuse and recycling store shall be submitted to and approved in writing by the Local Planning Authority. No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement, except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

Pre occupation condition(s)

13. Energy and Sustainability in accordance with statement

The development hereby approved shall incorporate the energy efficiency measures, renewable energy, sustainable design principles and climate change adaptation measures into the design and construction of the development in full accordance with the approved Climate Change and Sustainability Statement (ref. OC 1706231, by Environomic, dated 10 August 2017) prior to occupation. This shall include the provision of on-site renewable energy generation (Solar PV) adequate to achieve a 22% reduction in CO2 emissions beyond Part L 2013 Building Regulations.

Reason: To ensure the development incorporates measures to minimise the effects of, and can adapt to a changing climate in accordance with policies BCS13 (Climate Change), BC14 (sustainable energy), BCS15 (Sustainable design and construction), DM29 (Design of new buildings)

14. Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development, it must be reported immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with DEFRA and the Environment Agency's Model Procedures for the Management of Land Contamination, CLR 11, and where remediation is necessary a remediation scheme must be prepared which ensures the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is to be submitted to an approved in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

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15. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

16. Obscured Glazing

The west-facing windows at first and second floor levels shall be glazed with obscured glass, as set out on drawings 1619(L)06 rev F and 1619(L)07 rev F, prior to the occupation of the development hereby approved and retained and maintained as such thereafter.

Reason: In the interests of residential amenity.

17. Widening of vehicle crossover - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until drop kerbs have been installed at the carriageway edge and the vehicle cross-over across the footway fronting the site have been widened in accordance with the approved plans. An Excavation Licence (Section 171 Licence) must be obtained before starting any work on the adopted highway.

Reason: In the interests of pedestrian safety and accessibility

18. Completion and Maintenance of Car/Vehicle Parking - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the car/vehicle parking area (and associated turning space which shall be clearly demarked as a no-parking area) shown on the approved plans has been completed, and thereafter, the area shall be kept free of obstruction and available for the parking of vehicles associated with the development.

Reason: To ensure that there are adequate parking facilities to serve the development constructed to an acceptable standard.

Post occupation management

19. Restriction of Use of Roof

The first floor roof area to the north and west of the second floor element of the building shall not be used as a balcony, roof garden or similar amenity area without the grant of further specific planning permission from the Local Planning Authority.

Reason: To safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

20. Noise from plant & equipment

The rating level of any noise generated by plant & equipment as part of the development shall be at least 5 dB below the pre-existing background level as determined by BS4142: 2014 Methods for rating and assessing industrial and commercial sound.

Reason: To safeguard the amenity of nearby premises and the area generally

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21. Protection of Vehicular Visibility Splays

Nothing shall be erected, retained, planted and/or allowed to grow at or above a height of 0.6 metres within the property frontage that would obstruct a visibility splay of 25 metres from 2.4 metres back from the back edge of pavement at the centre point of the vehicular access. The visibility splay shall be maintained free of obstruction at all times thereafter.

Reason: In the interests of highway safety.

List of approved plans

22. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

Drainage Strategy, received 6 April 2018
 Bat scoping survey, received 19 December 2017
 Climate change and sustainability statement, received 19 December 2017
 1619(L)14 Existing ground floor plan, received 19 December 2017
 1619(L)16 Existing rear elevation, received 19 December 2017
 1619(L)03 Existing site block plan, received 19 December 2017
 1619(L)01 Existing site plan, received 19 December 2017
 1619(L)02 Existing street elevation, received 19 December 2017
 1619(L)15 Existing street elevation, received 19 December 2017
 1619(L)04 Proposed site block plan, received 19 December 2017
 1619(L)10c Proposed east elevation, received 11 July 2018
 1619(L)06F Proposed first floor plan, received 24 July 2018
 1619(L)05E Proposed ground floor plan, received 24 July 2018
 1619(L)12d Proposed north elevation, received 11 July 2018
 1619(L)08E Proposed roof plan, received 24 July 2018
 1619(L)07F Proposed second floor plan, received 24 July 2018
 1619(L)09C Proposed site plan, received 24 July 2018
 1619(L)11c Proposed south elevation, received 11 July 2018
 1619(L)13e Proposed west elevation, received 11 July 2018
 1619(L)00 Location plan, received 19 December 2017

Reason: For the avoidance of doubt.

Advices

1 Living roofs:

The roofs should be covered with local low-nutrient status aggregates (not topsoil) and no nutrients added. Ideally aggregates should be dominated by gravels with 10 - 20% of sands. On top of this there should be varying depths of sterilised sandy loam between 0 - 3 cm deep. An overall substrate depth of at least 10 cm of crushed demolition aggregate or pure crushed brick is desirable. The roofs should include areas of bare ground and not be entirely seeded (to allow wild plants to colonise) and not employ Sedum (stonecrop) because this has limited benefits for wildlife. To benefit certain invertebrates the roofs should include local substrates, stones, shingle and gravel with troughs and mounds, piles of pure sand 20 - 30 cm deep for solitary bees and wasps to nest in, small logs, coils of rope and log piles of dry dead wood to provide invertebrate niches (the use of egg-sized pebbles should be avoided because gulls

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and crows may pick the pebbles up and drop them). Deeper areas of substrate which are at least 20 cm deep are valuable to provide refuges for animals during dry spells. An area of wildflower meadow can also be seeded on the roof for pollinating insects. Please see www.thegreenroofcentre.co.uk and <http://livingroofs.org/> for further information and the following reference: English Nature (2006). Living roofs. ISBN 1 85716 934.4

- 2 Works on the public highway: The development hereby approved includes the carrying out of work on the public highway. You are advised that before undertaking work on the highway you must enter into a formal agreement with the council which would specify the works and the terms and conditions under which they are to be carried out. You should contact You should contact TDM - Strategic City Transport (100TS), Bristol City Council, PO Box 3176, Bristol, BS3 9FS, telephone 0117 903 6846 or email TransportDM@bristol.gov.uk, allowing sufficient time for the preparation and signing of the agreement. You will be required to pay fees to cover the councils cost's in undertaking the following actions:
 - 1) Drafting the agreement
 - 2) A monitoring fee equivalent to 15% of the planning application fee
 - 3) Approving the highway details
 - 4) Inspecting the highway works.

- 3 The development hereby approved is likely to impact on the highway network during its construction. The applicant is required to contact Highway Network Management to discuss any temporary traffic management measures required, such as footway, Public Right of Way or carriageway closures, or temporary parking restrictions. Please call 0117 9036852 or email traffic@bristol.gov.uk a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary Traffic Management measures to be agreed.

Supporting Documents

3. 17 Bridge Walk

1. Site location plan
2. Proposed ground floor & site plan
3. Proposed first floor plan
4. Proposed second floor plan
5. Proposed roof plan
6. Proposed site plan
7. Proposed East elevation
8. Proposed South elevation
9. Proposed North elevation
10. Proposed West elevation




SITE OUTLINE

REV	Note	Date
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Project **17 Bridge Walk,
 Bristol**

Drawing Title **Location Plan**

Drawing No. **1619(L)00**

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Scale @A4	drawn by	Date	Rev
1:1250	LH	05/05/16	-

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REV	Note	Date
E	Bin/bike store moved, internal layout updated	23.07.18
D	Building footprint moved, internal layout updated	13.06.18
C	Units 2 and 3 updated, garden updated, refuse updated	07.03.18
B	Notes added	05.09.17
A	Stair updated, Flat 2 amended	31.08.17

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Project **17 Bridge Walk, Bristol**

Drawing Title **Proposed Ground/Site Plan**

Drawing No. **1619(L)05**

Scale @A3 drawn by Date Rev
 1:100 LH 06/05/16 E



No.15

No.25C&D

NO PARKING

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REV	Note	Date
F	Bin/bike store moved, internal layout updated	23.07.18
E	Building footprint moved, internal layout updated	13.06.18
D	Units 5 and 6 updated	07.03.18
C	Balconies amended, window updated	20.11.17
B	Notes added	05.09.17
A	Stair updated, Flats 5+6 amended	31.08.17

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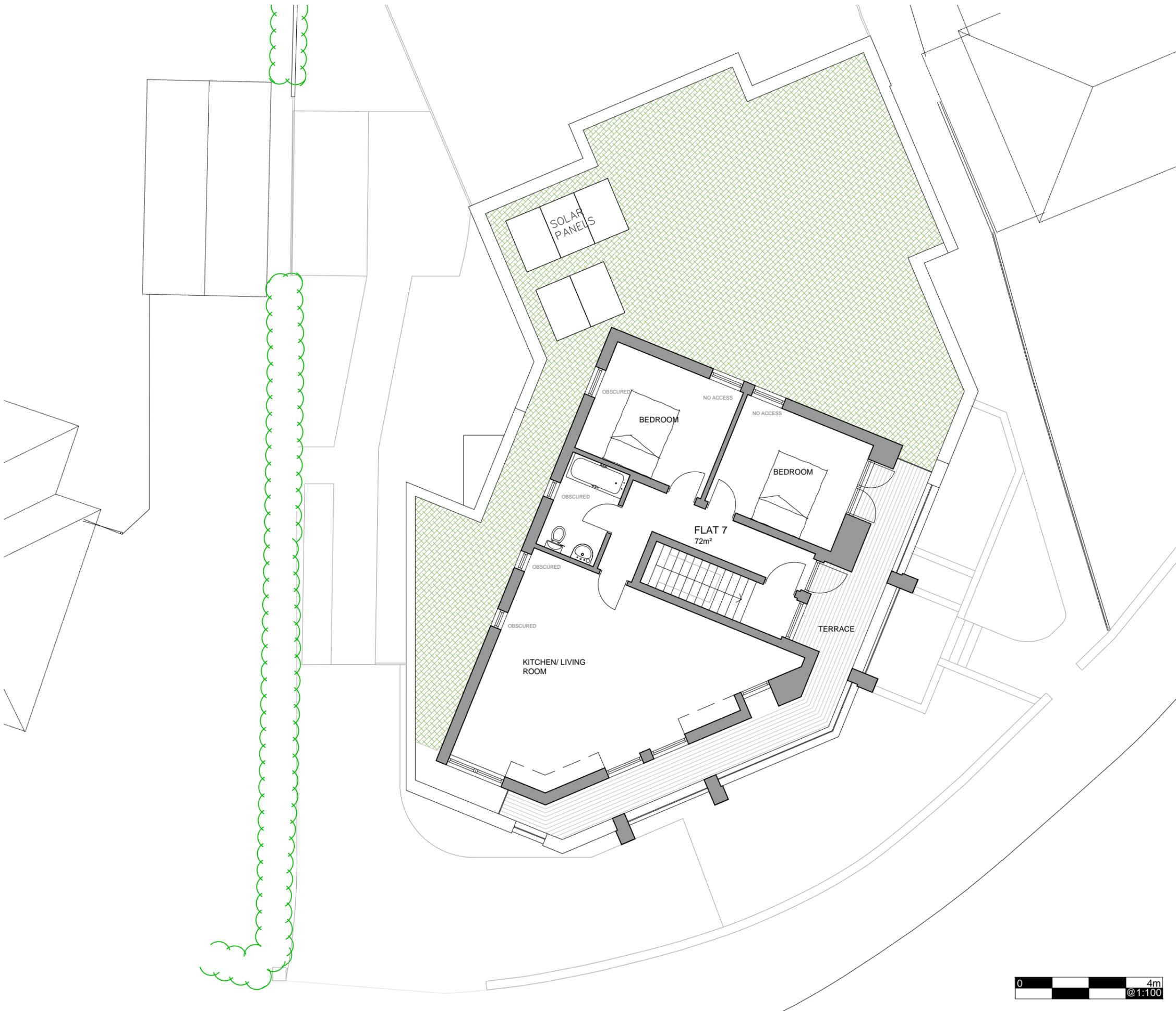
Project **17 Bridge Walk,
Bristol**

Drawing Title **Proposed First Floor
Plan**

Drawing No. **1619(L)06**

Scale @A3 drawn by Date Rev
1:100 LH 06/05/16 F

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REV	Note	Date
F	Bin/bike store moved, internal layout updated	23.07.18
E	Building footprint moved, internal layout updated	13.06.18
D	Unit removed, windows updated	07.03.18
C	Balconies updated	20.11.17
B	Notes added	05.09.17
A	Dwg scale updated, Stairs updated, Flats 6+7 amended	31.08.17

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Project **17 Bridge Walk, Bristol**

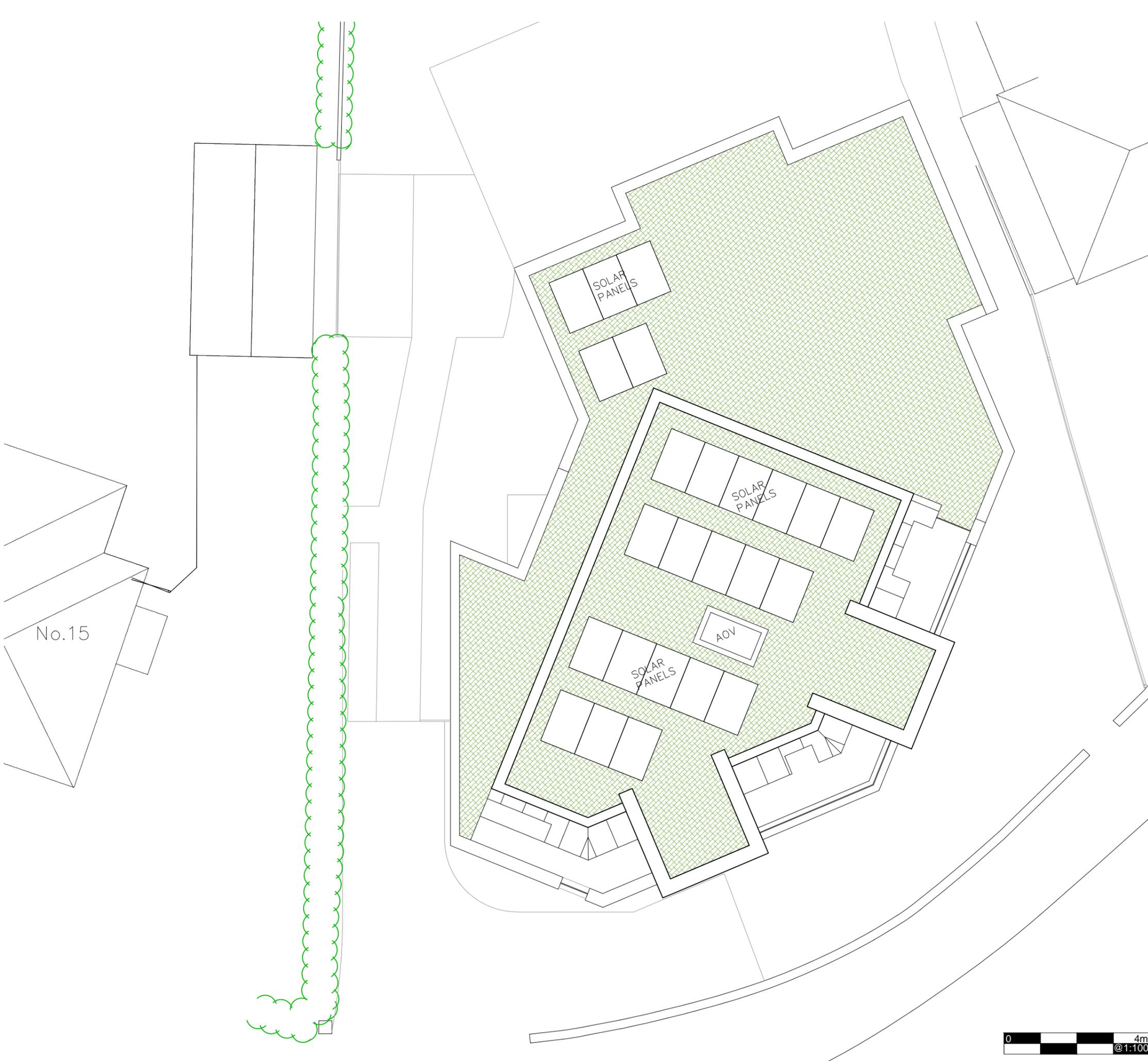
Drawing Title **Proposed Second Floor Plan**

Drawing No. **1619(L)07**

Scale @A3 drawn by Date Rev
1:100 LH 06/05/16 F



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REV	Note	Date
E	Bin/bike store moved, bay window updated	23.07.18
D	Building footprint moved, internal layout updated	13.06.18
C	Roof updated to new layout	07.03.18
B	Balconies updated	20.11.17
A	Scale Amended	06.09.17

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Project **17 Bridge Walk,
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Drawing Title **Proposed Roof/Site
Plan**

Drawing No. **1619(L)08**

Scale @A3 drawn by Date Rev
1:100 LH 06/05/16 E



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 PERMEABLE TARMAC

REV	Note	Date
C	Bin/bike store moved, internal layout updated	23.07.18
B	Building footprint moved, internal layout updated	13.06.18
A	Landscaping and parking updated	07.03.18

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Project **17 Bridge Walk,
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Drawing Title **Proposed Site Plan**

Drawing No. **1619(L)09**



Scale @A3 drawn by Date Rev
 1:200 LH 06/05/16 C

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Proposed East Elevation (in front of fence)

MATERIALS
 WINDOWS: COMPOSITE TIMBER AND ALUMINIUM WINDOWS (DARK GREY)
 WALLS: RED COLOURED BRICK TO GROUND AND FIRST FLOOR, GREY ZINC CLADDING TO PITCHED SECTIONS OF SECOND FLOOR, TIMBER VERTICAL CLADDING TO SECOND FLOOR
 ROOF: SINGLE PLY MEMBRANE AND EXTENSIVE GREEN ROOF
 DOORS: COMPOSITE TIMBER DOOR (DARK GREY)



Proposed East Elevation (From Road)

REV	Note	Date
C	Building moved and facade updated	13.06.18
B	Second floor reduced, garden amended	07.03.18
A	Balconies and Windows updated	20.11.17

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Project **17 Bridge Walk, Bristol**
 Drawing Title **Proposed East Elevation**

Drawing No. **1619(L)10**

Scale @A3 drawn by Date Rev
 1:100 LH 14/06/17 C



Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.

MATERIALS
 WINDOWS: COMPOSITE TIMBER AND ALUMINIUM WINDOWS (DARK GREY)
 WALLS: RED COLOURED BRICK TO GROUND AND FIRST FLOOR, GREY ZINC CLADDING TO PITCHED SECTIONS OF SECOND FLOOR, TIMBER VERTICAL CLADDING TO SECOND FLOOR
 ROOF: SINGLE PLY MEMBRANE AND EXTENSIVE GREEN ROOF
 DOORS: COMPOSITE TIMBER DOOR (DARK GREY)



Proposed South Elevation (in front of wall)



REV	Note	Date
C	Building moved and facade updated	13.06.18
B	Second floor updated, garden and wall updated, refuse removed	07.03.18
A	Balconies and Windows updated	20.11.17

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Project **17 Bridge Walk, Bristol**
 Drawing Title **Proposed South Elevation**

Drawing No. **1619(L)11**

Scale @A3 drawn by Date Rev
 1:100 LH 14/06/17 C



Proposed South Elevation (from the road)

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Proposed North Elevation



Proposed East Elevation (showing patio area)

REV	Note	Date
D	Building moved, facade updated	13.06.18
C	Second floor updated, garden updated	07.03.18
B	Balconies amended	20.11.17
A	Screen amended	07.09.17

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Project **17 Bridge Walk,
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Drawing Title **Proposed North
 Elevation**

Drawing No. **1619(L)12**

Scale @A3 drawn by Date Rev
1:100 LH 14/06/17 D

MATERIALS
 WINDOWS: COMPOSITE TIMBER AND ALUMINIUM WINDOWS (DARK GREY)
 WALLS: BUFF COLOURED BRICK TO GROUND AND FIRST FLOOR, BLACK
 TIMBER CLADDING TO SECOND FLOOR
 ROOF: SINGLE PLY MEMBRANE
 DOORS: COMPOSITE TIMBER DOOR (DARK GREY)



Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Proposed West Elevation

MATERIALS
 WINDOWS: COMPOSITE TIMBER AND ALUMINIUM WINDOWS (DARK GREY)
 WALLS: BUFF COLOURED BRICK TO GROUND AND FIRST FLOOR, BLACK
 TIMBER CLADDING TO SECOND FLOOR
 ROOF: SINGLE PLY MEMBRANE
 DOORS: COMPOSITE TIMBER DOOR (DARK GREY)

REV	Note	Date
C	Balconies amended	20.11.17
B	Screen amended	07.09.17
A	Bike store amended	22.08.17

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E	Building moved, facade updated	13.06.18
D	Roof amended, landscaping updated	20.11.17



Project **17 Bridge Walk,
 Bristol**

Drawing Title **Proposed West
 Elevation**

Drawing No. **1619(L)13**

Scale @A3 drawn by Date Rev
1:100 LH 14/06/17 E